

MEETING SUMMARY

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| Project: | Federal Boulevard Multimodal Transportation Study |
| Subject: | Joint Stakeholder and Technical Working Group Meeting |
| Meeting Date: | Wednesday, February 24, 2021 |
| Location or Call In: | Virtual meeting via 'Webex' (see electronic meeting calendar invitation) |

Meeting Purpose

This was the third meeting of the study's two working groups (Stakeholder Working Group and Technical Working Group).

The purpose was to continue to engage with the stakeholder teams, provide a street view of the before and after of other similar corridors, and review a summary of draft modal recommendations based on the outreach and findings conducted to date. An initial 'look' at a potential cross-section would be presented, and the general level of impacts would be discussed in small break-out groups. The primary goal was to solicit input from the SWG to the proposed cross-section balanced against potential impacts.

Welcome and Introductions

Tara Bettale, Consultant Team (HDR), welcomed the group and reviewed meeting logistics.

Chris Primus, Consultant Project Manager (HDR), discussed the overall agenda, noting that this was the third SWG meeting.

Tara then led a virtual sign-in using the chat function within Webex. Meeting attendees entered their names and respective organizations if applicable. Thirty-eight (38) individuals attended the meeting (see Attachment A of this meeting summary). The meeting presentation is also included as Appendix B.

Similar Corridors - Before and After Improvements

Keith Borsheim, Consultant Team (HDR), reviewed before and after slides depicting similar corridors around the United States and Canada. Images of similar corridors can be found in Appendix B of these notes.

- Examples of elements that Federal Blvd may want to include such as: public art, detached sidewalks, consistent lighting, bus lanes, pedestrian crossings, station upgrades, low-cost crossing enhancements.

Draft Modal Visions and Potential Elements

How stakeholder input has informed draft recommendations

Chris Primus reviewed the draft modal visions and potential elements to be incorporated into Federal Boulevard. This information included pros and cons of each element. He also provided context for how stakeholder input has informed these decisions.

Chris reminded the group of the project goals and pointed how the team built upon these goals to generate recommendations for the corridor.

Stakeholder Questions:

- (Regarding transit) Question from Tina Francone (Jefferson County): Who incurs the maintenance expense for the Stop/station enhancements?
 - Answer: Would be determined. Probably a joint expense between the cities and RTD through intergovernmental expenses.
 - Debra Baskett (Westminster) said that sometimes it is a Public/Private partnership.
 - Doug Monroe (RTD) said a regular bus stop is maintained by the property owner, accessible boarding area. When you get into amenities, then they are maintained by the owner of the amenities (i.e. advertising).
- Question: What is the cost difference between the 8' wide and the 10' wide sidewalk, including acquisition of ROW?
 - Debra said this is dependent on the ROW and the drainage.
- Question: Are trails the responsibility of the County's Open Space or City of Westminster?
 - Answer: Depending on the location, trail responsibility could be a responsibility of Adams County, the city it's located in, private development or a partnership.
- Question/Comment: Thinking about Zone A, where these improvements would be very welcome indeed. Question is about needed ROW and impacts to the businesses -- a number are mom and pop, many of Latinx, Asian, or other minority ownership/management. Can a sort of consideration be extended, flexibility in design and such, to preserve parking or otherwise limit impacts? Or is Federal sufficiently wide to accommodate much of this?
 - Answer: Great question. This will be the discussion in breakout groups.
- Comment: Lowell was just redone in unincorporated Adams County to 64th and included a wide multi-use sidewalk on the west side of the street. How do we make use of what was just done with some additional enhancements, and plan for additional

improvements in the future? I would hope that we can look at what can be fit into the existing ROW and repurposing what's there, versus expanding the ROW.

- Chris Chovan (Adams County) said he has been in conversations over several months about how to make a smooth transition.
- Kristina Evanoff, (Westminster), Comment: The Westminster Transportation and Mobility Plan is also developing a bike network at this time - so we'll be coordinating with the recommendations for Federal Blvd as well.

Cross Sections and Right-of-Way

Keith Borsheim (HDR) shared and led a discussion of the proposed cross sections. Specific cross sections can be found in Appendix B.

Stakeholder questions:

- Question: How were zones determined?
 - Answer: By compiling the general characteristics of each area. For example, roadway width, adjacent land uses, logical breakpoints. Needed to have a common look within each zone.
- Question: Will trucks travel in the two auto lanes in each direction of travel?
 - Answer: Yes
- Debra commented that the team worked hard to pick the boundaries to make the intersections remain whole.

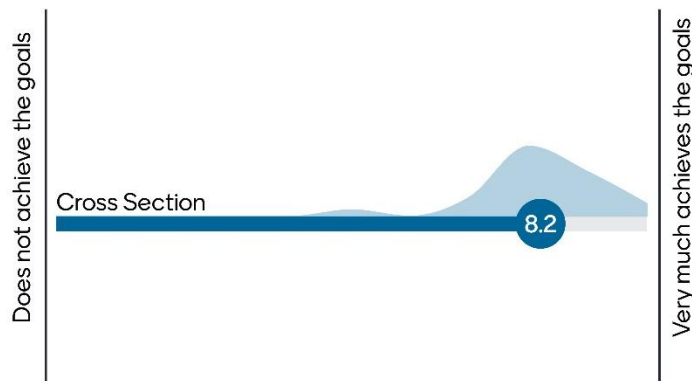
Menti Exercise: Stakeholder Feedback

Holly Buck, Consultant Team (FHU), led a stakeholder feedback activity which was hosted via Mentimeter. The goal was to get input from the stakeholders regarding the cross section shared earlier in the meeting and their preferences regarding long term expansion of the right-of-way (ROW) vs. avoiding property impacts.

Results from the Menti exercise is shown below.

To what degree do you feel that the shown cross section helps to achieve the goals outlined for Federal Boulevard?

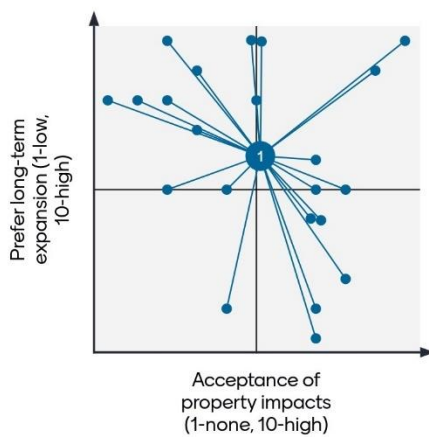
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Your preference: long-term expansion of the ROW vs. avoiding property impacts

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1 Please rank your preference

23

Stakeholder Feedback: Breakout Groups

The team then conducted breakout groups via Webex to facilitate smaller group conversation, to discuss reactions to the Menti questions and the potential impacts along Federal Boulevard. Each breakout group was divided by specific sections along Federal Boulevard, as noted.

Breakout Group: Zone A (52nd Avenue - 72nd Avenue)

Chris asked the group their thoughts on the Menti results:

- Annemarie Heinrich Fortune (Tri-County Health Department): The interpretation of the second Menti poll is spot on - really seems like tolerance for right-of-way depends on where you are at in the corridor.
- Doug Monroe (RTD): Agrees with Annemarie. He is in favor of taking the ROW in areas where it's impacting parking lots so that we can improve transportation throughout the corridor but wants to be considerate of property and business owners as well.
- Jenna Farley (Regis University): Appreciated the cross-section renderings.
- Joe DeMers (Hyland Hills): Reiterated the need for a case-by-case sensitivity towards ROW and the businesses along the corridor.
- Shelley Cook (RTD): Liked the characterization. From an RTD perspective and as a pedestrian who often walks in the project corridor, the potential improvements are great. But, for "mom and pop" businesses, parking is critical so would like to find a way to rectify that.

Chris Primus then showed the roll plot of high-level impacts along Federal Boulevard for Zone A. Discussion regarding the impacts is captured below.

- Stakeholder question: does this plan call for reducing the curb cuts?
 - Chris explained: it is too early to say - the hope would be to consolidate cutouts for the sake of safety. This will be determined on a case by case basis. The fewer driveway accesses the better from a safety standpoint but would need to determine the use for properties along the corridor.
- Shelley Cook
 - A project is underway in Arvada called Ralston Road Phase I. Ralston is one of the oldest roads around. There is limited ROW and prior lane widths were as small as 9 feet (a scary experience walking next to the cars, not to mention the bus mirrors). The project includes widening, adding wide sidewalks, and maybe some bike lanes.
 - Businesses are currently hugged, sort of, by the sidewalks.
 - The project extends almost up to some of the structures, but it seems fine and, once completed with landscaping, looks like it will be great.

Chris asked: are the type and nature of impacts acceptable to achieving the complete street cross section?

- Stakeholders did not answer this question.

Chris asked: would the addition of a transit semi-dedicated lane help meet the transit goals identified for the project?

- Doug Monroe - From RTD's perspective, yes! Would certainly help the efficiency. Especially because there are two rail stations in the vicinity, this would help with those connections.
 - He also pointed out that Zone A reflects the highest transit ridership in this corridor
- Shelley Cook - agrees with Doug's point.
- Jenna Farley - often hears concerns from fire department/emergency services regarding the reconfiguration? She is wondering if there is validity to these concerns.
 - Chris explained that this is a good question and though some fire department and EMS representatives are invited to these stakeholder meetings, there are not any present in this stakeholder group. Noted that we will look into this and identify the answer to this question
 - Doug added that on similar projects in Denver, EMS and fire departments have been very happy and semi-dedicated transit lanes have offered improvements for them as well.
- Jenna Farley - asked if there is opportunity to partner with the Federal Blvd work that is going on within City and County of Denver (CCD), with the goals to connect improvements. Would be great to tie in consistent and similar improvements along the corridor.
 - Doug noted that City and County of Denver's Federal Blvd BRT project is envisioning an integrated system with Federal Blvd into the City of Westminster, Adams County, and Federal Heights

Chris asked for any final comments/questions for the breakout group:

- Shelley Cook is very happy with the identified improvements overall and hopes it can be implemented. Made the point that as a bicyclist, Clear Creek to G-line to Lowell is challenging. Especially because there are improved bicycle facilities along Lowell now, so more incentive to ride along Lowell. How would you travel as a bicyclist from Clear Creek/Federal Blvd to Lowell? Recommended the project team might look into this.
- Annemarie Heinrich emphasized the importance of business and property owners along the corridor. She would like to see the safety improvements for pedestrians/etc. but

hopes to find a way of doing so that will not cause economic strain on business owners.

Breakout Group: Zone B (72nd Avenue - 80th Avenue)

Tara Bettale began conversation and the group agreed that the Menti results are not terribly surprising. The group agreed that the typical section meets the goals of the corridor and specific ROW impacts need to be understood and considered further.

General responses:

- Mark Shuman (Adams County Fire Rescue) - as a first responder, he recognizes he has a different response from the typical. Mentioned that the increased safety is great. Some concern for the raised medians which can be a launching hazard and make turning emergency vehicles around difficult. Also complicates first responder routing - requiring U-turns or navigating side streets to get to a destination.
- Kristin Sullivan (Adams County) - 10' detached walk is a high priority. Pedestrians lack a safe place on Federal today, and this represents a much-needed change. Also conveys a strong connection with the goal of saving lives, so safety for all on the corridor.
- Deya Zavala (Mile High Connects) - lives in the corridor and is thrilled as a resident. As a representative of Mile High Connects shared excitement for improved amenity zones. Shared some hesitancy about significant parking impacts impacting small businesses on the corridor.
- Charlie Dyrsten (HDR) answered a question that lighting is included in these amenity zones - both roadway and pedestrian lighting.
- Deya Zavala - excited about BRT on the corridor. This is very important to the Mile High Connects community.

Roll Plot and additional discussion

- Tara walked through the roll plot and Zone B and prompted: At what point do we need to sacrifice something for the multimodal section?
- Kristin Sullivan noted that in this section, the parking impacts are minimal and parking needs may be met in a variety of other locations - side streets, new parking alignment on the property, etc.
- Asked about the structure impacts in B - Charlie mentioned that these are garages and sheds at the back of folks' properties near the ROW line. The alignment of the roadway could be shifted to increase parking impacts and avoid these impacts.
- Tara mentioned that this kind of tradeoff will continue to be investigated as the project moves forward.

- Tara asked about the US-36 interchange - Charlie responded that this Federal Blvd Multimodal Study is currently looking at changes near the Turnpike interchange at a high level, which will need to be refined and would need CDOT approval to implement.
- Kristin Sullivan mentioned the possibility of interim improvements to pedestrian infrastructure over US-36 with improved crossings before/after, with an eventual lens of the ultimate section. The sentiment is characterized as “don’t let perfect be the enemy of the good” - Charlie responds that this is in line with the intended use of the plan. This will give the corridor an ultimate section to work toward, and incremental and partial improvements are needed and beneficial.
- Tim Williams (Federal Heights) - It’s also good to look to build this section as much as possible in one iteration. This reduces investment needed in development (who would otherwise improve just the parcel that they develop on) and provides a much more consistent corridor.

Tara prompted about transit improvements and goals.

- Kristin Sullivan - transit focus is good. Be prepared for opposition to taking a lane from general purpose traffic. Federal is often an I-25 alternate route. With the other improvements likely leading to slower speeds, is there an anticipated change in transit ridership on the corridor? (Unanswered question)
- Deya Zavala - We’ve talked about this as a multimodal project, and the peds and transit are apparent. Where are bikes supposed to be? Tara mentions that the 10’ shared use path is wide enough to allow for both pedestrians and bikes. With the limited space that we have, the shared use path was the solution on this corridor, as compared to on-street bike lanes which would require additional roadway width. Lowell is the parallel and more comfortable North/South facility.

Tara prompts for closing thoughts and asks if any technical data is needed.

- Mark Shuman - really appreciates being involved and no technical needs at this time. Access to properties is important - reiterates that for emergency vehicles, the corridor needs to have frequent turn around locations, or they’ll have to use side streets.
- Kristin Sullivan - thumbs up.
- Deya Zavala - all good.

Chat comments

- Kristin Sullivan: In some sections of Federal, the lighting is in the median so we would want to look at transitions into that section or modifications to improve consistency.

Breakout Group: Zone C (80th Avenue - 104th Avenue)

Introduction: Holly Buck (FHU) introduced the zone endpoints (81st Ave to 105th Ave), and went through the high-level description of the zone.

Reactions to Menti Questions:

- Menti Question 1 - no significant comment
- Menti Question 2 - long-term impacts:
 - Jeff Hill (Federal Heights) noted difficulties of conveying changes to the public - need to help community members understand tradeoffs.
 - Kristina Evanoff noted need to explain difference in use of ROW in non-urban setting (such as this) - explain to community what they might be able to expect from changes.
- Conversation about tradeoffs (initiated by Kristina):
 - Holly noted difficulty of balancing competing interests - the project team has worked to accommodate vehicle traffic while enhancing multimodal options as well as safety.
- Renae Stravos (Federal Heights) - referencing 92nd Avenue improvements, need to re-plan parking lots for that project. She noted the difficulty/effort that went into making those changes, even in an environment where there was relatively little push-back on ROW impacts (given that the takes were limited to parking lots)
 - Follow-up discussion noted access changes needed - Renae discussed efforts to accommodate change with navigation, orientation of spaces, etc. to maximize existing space.
 - Jeff noted that the project involved creating variances to things like landscaping requirements.
- Debra Baskett brought up question from RTD board member on possible impacts to disadvantaged populations/businesses. Noted the difficulty of making those connections (with businesses in particular) during COVID, and the importance of keeping those stakeholders involved in the discussion.

Roll-plot discussion

- Debra offered context on opportunities in the vicinity of 86th Avenue (to the west of Federal) - possible land development, as well as pedestrian crossing near school at 86th (short-term improvement).
- Jason Igo (CDOT) clarified that CDOT is supportive of HAWK signals in locations like 86th, where signal spacing is longer than desired.
- Kristina asked about phasing/timeline (near-term/long-term) - Holly clarified that the roll-plot visual depicts a possible long-term cross-section, while short-term

improvements are at least partially captured as part of CDOT's ongoing repaving effort.

- Jeff Hill noted the safety issues presented by offset lanes at 88th - pushed for improvements to address the problem (operationally or with infrastructure changes). Debra noted that this intersection is certainly a key focus of the project - and that the land developer at that intersection is hopefully going to be a participant in the fix of that issue. Jeff proposed a controlled left turn as an interim solution, given the uncertainty associated with developers.
 - Holly noted that the project team would make sure that signal timing and related improvements are considered as part of short-term solutions.
- Discussion further north included consideration of parking impacts.
- Note that breakout discussion got just past 88th Ave - some consideration progressing north to 100th - Holly noted that there would be opportunities for follow-up discussion.

Additional Notes

- Property owners don't typically want to give up right of way.
- Need to demonstrate that the project will enhance the front of your property.
- There is an educational aspect of the project too - beyond the vehicles.
- ROW acquisition is challenging but having tradeoffs discussions is good.
- 92nd Avenue ROW acquisition required Federal Heights staff to redesign parking lots and signing. When curb cuts are modified, or parking is reduced, the circulation in a parking lot is often impacted.
- The group was curious if people are coming to the businesses from local areas or farther away? This would impact how much parking they need - Can they bike or walk instead with these improvements?
- 92nd also required variances on the landscape requirements because they couldn't fit in the available right-of-way.
- Signal control at 88th Avenue should be reviewed, and the intersection needs to be realigned to eliminate the offset intersections.
- Developer at 88th will be required to build sidewalk in accordance with recommendations for the ultimate corridor.

Breakout Group: Zone D (104th Avenue - 120th Avenue)

Keith Borsheim led the conversation, beginning with a discussion of overall reactions to the Menti questions.

Response from the Menti Polls

- Adam Parks (CDOT) noted that we are aware of how constrained the ROW is further south in the corridor, because of the curb ramp replacements project. We are mapping the ROW because of this project. We'll start with the most troublesome spots curb ramps and missing connections.
- Carson Priest (SMART Commute) noted that ROW is a nightmare down south.
- Matthew Helfant (DRCOG) - Liked the multipurpose trail and trail connections as these would help pedestrians throughout the corridor.
- Tina Francone noted that it is no question that there are issues with this corridor. All suggestions look good but is concerned about how much it'll cost and who would be responsible for it. Also noted that getting a commitment from RTD to increase ridership along this corridor would be critical. Most jurisdictions roll the ADA compliance into their general maintenance; when curbs/gutters need maintenance or replacement, that's when the ADA improvements occur.
 - She asked if there is funding to put more buses out there? Keith noted that RTD is having trouble getting funding.

Keith presented the roll plot map.

Keith explained that Zone D includes 120th down to 105th Avenues. He showed some of the impacts, Detached sideways. A little more pavement for much of the corridor.

Overall summary - This segment doesn't have many concerns. There is general support and a desire for quick implementation projects to address safety.

- Peip Van Heuven - Slip lanes become important.
- Keith should look out for that as an early action to identify high crash zones.
- Adam Parks - CDOT completed resurfacing project in 2018, so the pavement should be in good condition between 92nd-120th Avenues.

Discussion on Zone A

- More impacts through this zone, potential structure impacts.
- 54th Avenue reflects more potential impacts to “mom and pop shops” and potential parking impacts.
 - Adam - impacts would require a lot of interaction with buessiness/property owners along the corridor and likely will be difficult. Getting good pedestrian flow to businesses is typically a benefit.

- Tina noted that it looks like the impact would be minimal, nonetheless, it does involve the acquisition of a property.
- Peip asked what is the space needed for the Bus Rapid Transit and how will that impact single occupancy vehicles?

Keith asked if there is technical data or supporting documentation to support these types of improvements that stakeholders/agencies need.

- Adam noted that it is dependent on if the funding is there.
- Keith asked from a traffic operation standpoint, are there hurdles that are taller than usual?
- Adam recommended to lean on Jason Igo (CDOT). He is on the traffic side, would need a traffic analysis. There is lower usage of the outer land and what it would mean to do a road diet there.
- Tina noted that she would like to understand cost estimates.
- Matthew Helfant (DRCOG) noted that BRT from 128th to Santa Fe/Dartmouth is in DRCOG's Fiscally Constrained Plan to be adopted this Spring in the 2030-2039 staging period, which assumed there is funding for this project.
- Peip - If there was a mention of uniform application of bike parking in the general presentation, I missed it. I'd apply bike parking with some consistent formula - every 2 blocks, at transit stops, and key intersections with bike routes or multi-use trails.
- Tina agreed with an emphasis on bike parking.
- Peip - I like the suggestion of road diet in certain areas, as that is a good application to slow speeds and it solves the ROW/parking issue.
- Tina - Maybe even bike storage facilities next to public transportation, similar to the Mineral Light Rail Station?
- Peip - something visual to make people slow down such as imagery or rumble strips.
- Carson Priest (SMART Commute) - Nothing from a TDM standpoint. Happy with mobility improvements. Having a shared use path would be a big goal.
- Adam - Challenges will be for figuring out the correct solution for each segment.
- <https://www.hotcars.cm/20-weirdest-things-people-painted-on-roads-to-slow-down-speeding-cars/>

Next Steps

Chris Primus closed the meeting and thanked stakeholders for their feedback. He indicated that the next steps would include public outreach opportunities in the forms of a digital survey this spring and an online public meeting in late spring/early summer.

Appendix A: Attendee List

| Organization | First | Last |
|--|-----------|------------------|
| Adams County | Jill | Jennings Golich |
| Adams County | Kristin | Sullivan |
| Adams County | Libby | Tart |
| Adams County Fire Rescue | Mark | Schuman |
| Adams County PW | Chris | Chovan |
| Alto Terrace Gardens Community Council, Maiker Housing Partners | Linnea | Bjorkman |
| Apex Design | Carly | Macias |
| Bicycle Colorado | Piep | van Heuven |
| CDOT | Jason | Igo |
| CDOT | Adam | Parks |
| City of Federal Heights | Alex | Edwards |
| City of Federal Heights | Jeff | Hill |
| City of Federal Heights | Tim | Williams |
| City of Federal Heights | Renaë | Stavros |
| City of Westminster | Debra | Baskett |
| City of Westminster | Kristina | Evanoff |
| City of Westminster | Logan | Morley |
| DRCOG | Matthew | Helfant |
| FHU (Consultant Team) | Holly | Buck |
| HDR Inc. (Consultant Team) | Keith | Borsheim |
| HDR Inc. (Consultant Team) | Tara | Bettale |
| HDR Inc. (Consultant Team) | Cristina | Beermann |
| HDR Inc. (Consultant Team) | Chris | Primus |
| HDR Inc. (Consultant Team) | Charlie | Dyrsten |
| HDR Inc. (Consultant Team) | Kiernan | Malestky |
| HDR Inc. (Consultant Team) | Ameerah | Palacios |
| Hyland Hills P&R District | Joe | DeMers |
| Mile High Connects | Deyanira | Zavala |
| Project Vision 21 | Francisco | Miraval |
| Regis University | Jenna | Farley |
| Resident | Ann | K. Long |
| Resident | Selena | Shepard |
| RTD | Doug | Monroe |
| RTD | Shelley | Cook |
| SeniorHub | Stephanie | Knight |
| Smart Commute Metro North | Carson | Priest |
| Tri-County Health Department | Annemarie | Heinrich Fortune |

APPENDIX B: PRESENTATION

Working Group Meeting #3

Joint Technical and Stakeholder Working Groups

February 24, 2021



Meeting Facilitation

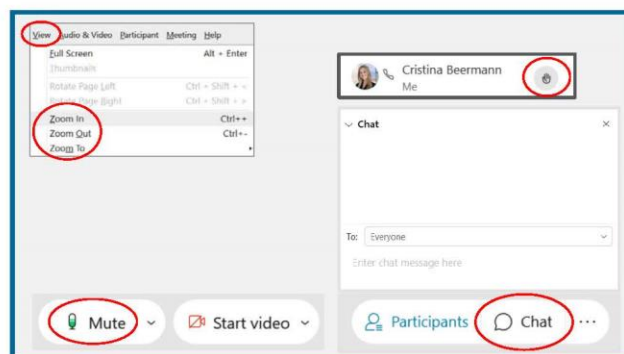
Ask Questions & Provide Comments

Webex Instructions

- Maintain mute
- Video
- Chat box
- Hand raise
- Zoom In/Out

Menti.com for feedback exercise

Breakout Group Prompts



Go to [menti.com](https://www.menti.com) and use the code: 30 69 96 5

Welcome

Welcome

Who We Are

- The 'Partnership' management team:
 - Adams County:
Chris Chovan
Senior Transportation and Mobility Planner
 - City of Federal Heights:
Tim Williams
Community Development Director
 - City of Westminster:
Debra Baskett
Senior Transportation and Mobility Planner



Additional Project Team Members

Presenters and Breakout Group Facilitators

- Chris Primus, Consultant Team
- Tara Bettale, Consultant Team
- Keith Borsheim, Consultant Team
- Holly Buck, Consultant Team
- Kiernan Maletsky, Consultant Team
- Cristina Beermann, Consultant Team
- Charlie Dyrsten, Consultant Team
- Rocio Ramirez, Consultant Team
- Kira Olson, Consultant Team

Working Group Members Roll Call...

Thank you for joining us today! Please 'sign-in' by entering your name, organization (if applicable), and which Corridor Zone you would like to discuss in today's breakout group discussion in the Chat box on the bottom right of your screen.



Today's Meeting

Agenda

- Introductions and Meeting Purpose
- Other Corridors – Before and After Improvements
- Draft Modal Visions and Potential Elements
- Cross Sections and Right-of-Way
 - Stakeholder feedback: breakout rooms
- Next steps
 - Digital survey
 - Public online meeting



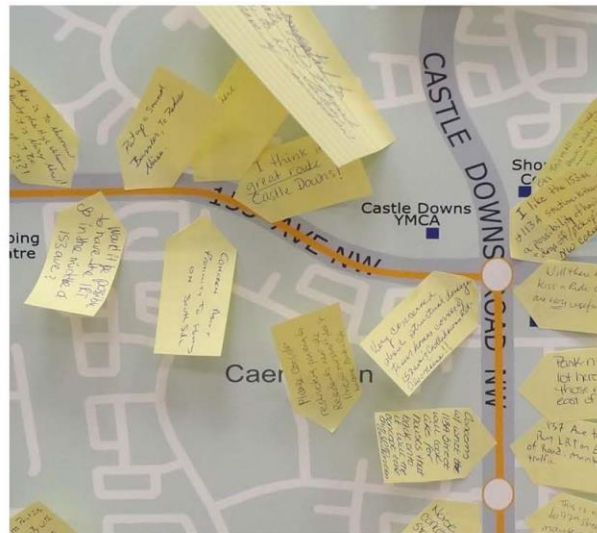
FEDERAL BOULEVARD
MULTIMODAL TRANSPORTATION STUDY

7

Meeting Purpose & Stakeholder Role

This Meeting Will

- Provide a project status update
- Share examples of similar corridor success stories
- Understand stakeholder feedback on cross sections and right-of-way
- Provide opportunity for input and questions



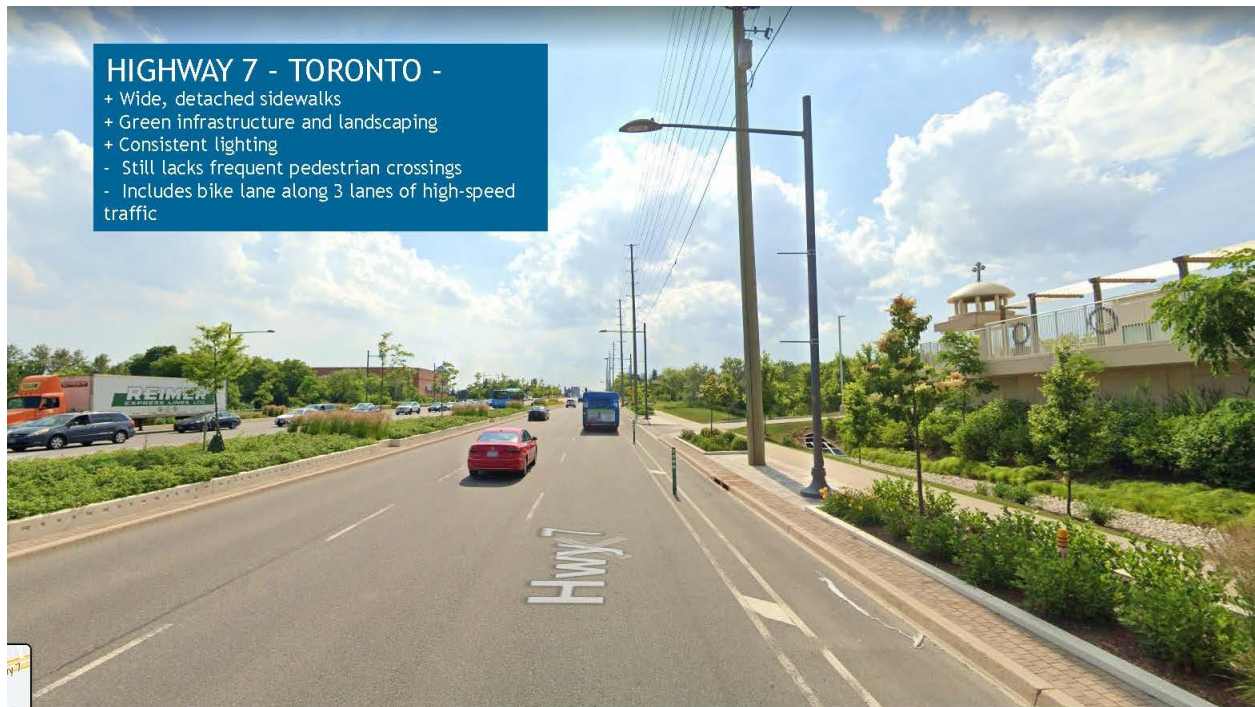
FEDERAL BOULEVARD
MULTIMODAL TRANSPORTATION STUDY

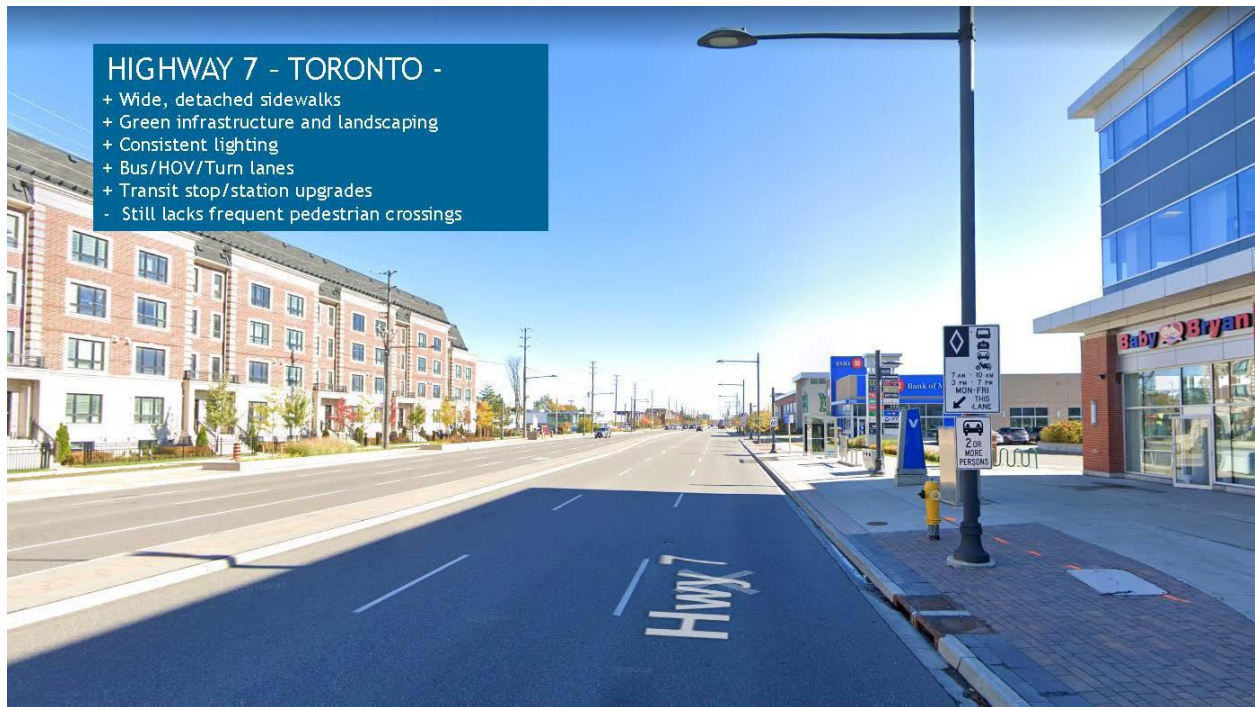
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Similar Corridors

HIGHWAY 7 - TORONTO - BEFORE







HIGHWAY 7 - TORONTO -

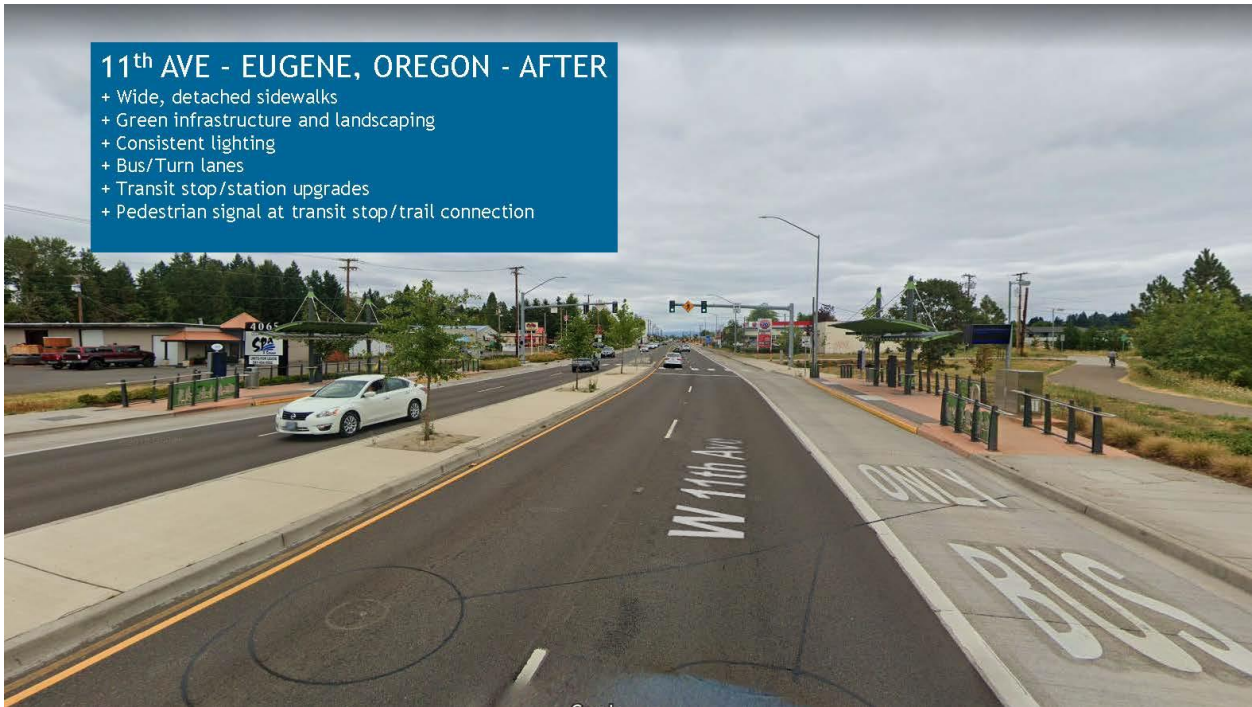
- + Wide, detached sidewalks
- + Green infrastructure and landscaping
- + Consistent lighting
- + Bus/HOV/Turn lanes
- + Transit stop/station upgrades
- Still lacks frequent pedestrian crossings



11th AVE - EUGENE, OREGON - BEFORE

11th AVE - EUGENE, OREGON - AFTER

- + Wide, detached sidewalks
- + Green infrastructure and landscaping
- + Consistent lighting
- + Bus/Turn lanes
- + Transit stop/station upgrades
- + Pedestrian signal at transit stop/trail connection



CENTRAL AVE - ALBUQUERQUE, NM - BEFORE



CENTRAL AVE - ALBUQUERQUE, NM - AFTER

- + Green infrastructure and landscaping in median
- + Upgraded lighting
- + Center-running bus rapid transit facilities
- + Transit stop/station upgrades
- + Incorporate public art



COLFAX & PARK AVE - BEFORE



COLFAX & PARK AVE - AFTER

- + Low-cost crossing enhancements
- + Major reductions in pedestrian-involved crashes





Draft Mobility Needs & Goals

The Federal Boulevard Multimodal Transportation Study will...

- Focus on **safe, equitable, consistent, and high-quality** options
- Build on **existing multimodal facilities** (trails, rail, etc.)
- Identify **attainable alternatives** to driving alone
- Generate recommendations for **walking, rolling, biking, transit, driving, and freight**

Draft Modal Visions & Potential Elements

Reference Document

Review the reference document for details

- Background data and findings
- Potential element pros and cons
- Recommendations

FEDERAL BOULEVARD
MULTIMODAL TRANSPORTATION STUDY

2017.03.07
Draft Multimodal Vision
Pedestrians

| | |
|---|---|
| <p>1. What are the existing conditions on Federal Boulevard?</p> <ul style="list-style-type: none"> • Average volume of traffic per week, morning, afternoon, and at night, based on current traffic volume. • Light pollution from street lighting. • The location of existing pedestrian crossings. • The location of existing pedestrian crossings. | <p>2. What was summarized by stakeholders?</p> <ul style="list-style-type: none"> • Current and future pedestrian needs. • Current and future pedestrian needs. • Current and future pedestrian needs. • Current and future pedestrian needs. |
| <p>3. What are the objectives for pedestrian facilities along Federal?</p> <ul style="list-style-type: none"> • Enhance the pedestrian experience. • Enhance the pedestrian experience. • Enhance the pedestrian experience. • Enhance the pedestrian experience. | <p>4. Why should pedestrian facilities be improved?</p> <ul style="list-style-type: none"> • Pedestrians are a vital part of the community. • Pedestrians are a vital part of the community. • Pedestrians are a vital part of the community. • Pedestrians are a vital part of the community. |

FEDERAL BOULEVARD
MULTIMODAL TRANSPORTATION STUDY

2017.03.07
Draft Multimodal Vision
Pedestrians

| Potential Elements | Pros | Cons | Recommendations |
|--|---|--|--|
| <p>Asphalt Sidewalk - 10' wide</p> | <ul style="list-style-type: none"> • Low cost, easy to install, and durable. • Provides a clear path for pedestrians. • Provides a clear path for pedestrians. | <ul style="list-style-type: none"> • Limited aesthetic appeal. • Limited aesthetic appeal. | <p>10</p> <ul style="list-style-type: none"> • Recommended for all streets. • Recommended for all streets. |
| <p>Decorative Sidewalk - 10' wide</p> | <ul style="list-style-type: none"> • Enhances the pedestrian experience. • Enhances the pedestrian experience. | <ul style="list-style-type: none"> • Higher cost than asphalt. • Higher cost than asphalt. | <p>11</p> <ul style="list-style-type: none"> • Recommended for all streets. • Recommended for all streets. |
| <p>Decorative Paved Path - 10' wide</p> | <ul style="list-style-type: none"> • Enhances the pedestrian experience. • Enhances the pedestrian experience. | <ul style="list-style-type: none"> • Higher cost than asphalt. • Higher cost than asphalt. | <p>12</p> <ul style="list-style-type: none"> • Recommended for all streets. • Recommended for all streets. |
| <p>Enhanced Safety for all Users at Intersections - various treatments</p> | <ul style="list-style-type: none"> • Improves safety for all users. • Improves safety for all users. | <ul style="list-style-type: none"> • Higher cost than asphalt. • Higher cost than asphalt. | <p>13</p> <ul style="list-style-type: none"> • Recommended for all streets. • Recommended for all streets. |
| <p>Other Pedestrian Crossing Signals, Beacons, and High Visibility Crosswalks</p> | <ul style="list-style-type: none"> • Improves safety for all users. • Improves safety for all users. | <ul style="list-style-type: none"> • Higher cost than asphalt. • Higher cost than asphalt. | <p>14</p> <ul style="list-style-type: none"> • Recommended for all streets. • Recommended for all streets. |

Pedestrians

Existing Conditions and Stakeholder Input

- Many pedestrian crashes
- Difficult to cross
- Important issue for most stakeholders

Objectives for Improvements

- Greater connectivity
- Encourage addressing sidewalk concerns
- Improve safety for pedestrians

Pedestrians



Attached Sidewalk - 8' wide



Detached Multi-use Path - 10' wide



Safer Pedestrian Crossings: Signals, Rectangular Rapid Flashing Beacons (RRFBs) & High Intensity Activated Crosswalks (HAWKs)



Detached Sidewalk - 8' wide



Enhanced Safety for all Users at Intersections - various treatments



Transit

Existing Conditions and Stakeholder Input

- Slower bus speeds due to congestion
- Record ridership
- Advocate for quality transit amenities
- Improve service

Objectives for Improvements

- Lack of multimodal connections to bus stops
- Safer access to bus stops and shelters
- More frequent service
- Prepare for ridership growth
- Opportunity for development, redevelopment, and land preservation

Transit



Semi-exclusive dedicated bus lanes (lanes that are reserved for buses and right turning vehicles)

| | Federal - 30th | Clear Creek - Federal Station - 40th St | Westminster Station | Federal - 72nd | Federal - 104th | Plover Range Community College | 108th & 90th - 90th St |
|----|----------------|---|---------------------|----------------|-----------------|--------------------------------|------------------------|
| 1 | 510A | 515A | 522A | — | — | — | — |
| 2 | 540A | 545A | — | 550A | 605A | — | 605A |
| 3 | 610A | 615A | 622A | — | — | — | — |
| 4 | 620A | 625A | 633A | — | — | — | — |
| 5 | 630A | 635A | 643A | — | — | — | — |
| 6 | 640A | 645A | — | 652A | 703A | 708A | — |
| 7 | 650A | 655A | 703A | — | — | — | — |
| 8 | 700A | 705A | 713A | — | — | — | — |
| 9 | 710A | 715A | 723A | — | — | — | — |
| 10 | 720A | 725A | 733A | — | — | — | — |

Service Enhancements - more frequent service (every 10 minutes), Timed Transfers to other routes and increased hours of service



Transit bypass lanes (dedicated lanes for bus approaches to intersections) at key locations, coupled with transit signal priority 'queue jumps'



Stop/Station Enhancements - ADA accessible bus shelters, relocated bus stops, sidewalk connections, multilingual information

Key

✓ Recommended corridor-wide

✗ Not Recommended

Bicycles

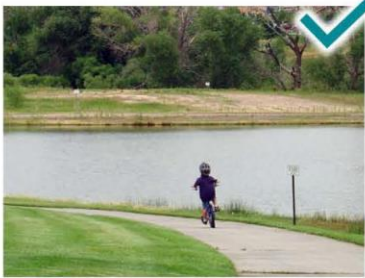
Existing Conditions and Stakeholder Input

- Minimal on-street bike infrastructure
- Not a designated bike corridor
- Encourage development of complete bike network
- Advocate for underpasses for all ages and abilities

Objectives for Improvements

- Concern over air quality
- More bike connections to neighborhoods
- Reduce emissions/pollution
- Addresses congestion management

Bicycles



Expanded connections to adjacent side paths



Dedicated Bike Lane along Lowell Boulevard



Dedicated Bike Lane along Federal Boulevard



Trails

Existing Conditions and Stakeholder Input

- Five regional trails
- Address missing greenway trail and infrastructure
- Encourage accessible connections
- Greater connectivity to existing trails

Objectives for Improvements

- Option for commuters
- Provide a connection to Camenisch Park
- Enhance connection to natural resources

Trails



Wayfinding



Expanded connections to existing regional trails



Connections to other modal options

Key



Recommended corridor-wide Not Recommended

Screeescapes

Existing Conditions and Stakeholder Input

- Lack of unifying streetscape elements
- No sidewalk uniformity
- Prioritize wayfinding to key destinations
- Reduce urban heat island effect

Objectives for Improvements

- Aesthetic consistency
- Tree canopy
- Enhances traffic calming
- Greater user comfort

Screeescapes



Street Lighting and Pedestrian Amenities



Green drainage elements



Brick or hardscape median



Public Art/Gateway Elements



Improved Signage - Inventory and reassessment



Auto Safety and Operations


Existing Conditions and Stakeholder Input

- Inadequate sight distance
- Non-compliant signals equipment
- Insufficient ADA facilities
- Improve traffic operations along the corridor
- Meet Vision Zero goals


Objectives for Improvements

- Manage congestion
- Vehicle speed
- Improve travel time
- Improve safety
- Reduce congestion

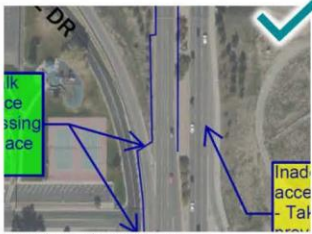
Auto Safety and Operations




Update signal equipment - Signal Rebuilds




Raised medians




Interchange improvements



Improved left turn treatments




ADA compliant design




Consistent corridor speed limits and signal progression based on lower target speeds

Key



Recommended
corridor-wide



Not
Recommended

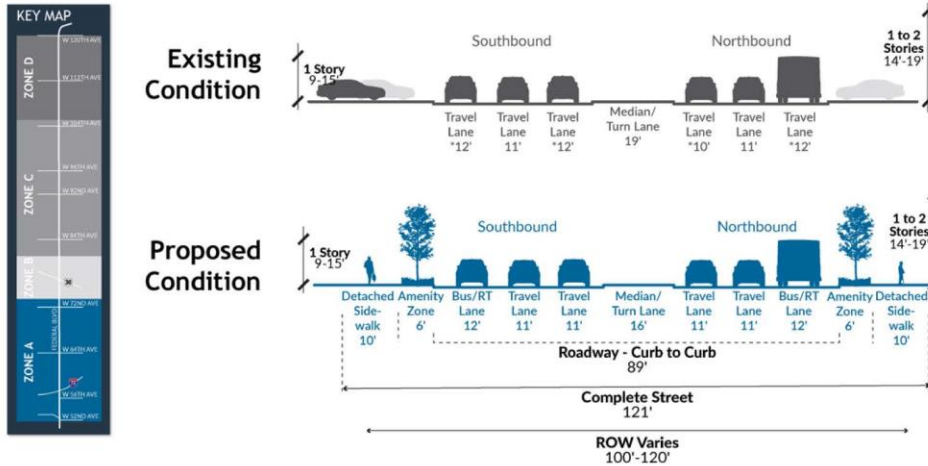
Cross Sections & Right-of-Way

Cross Section



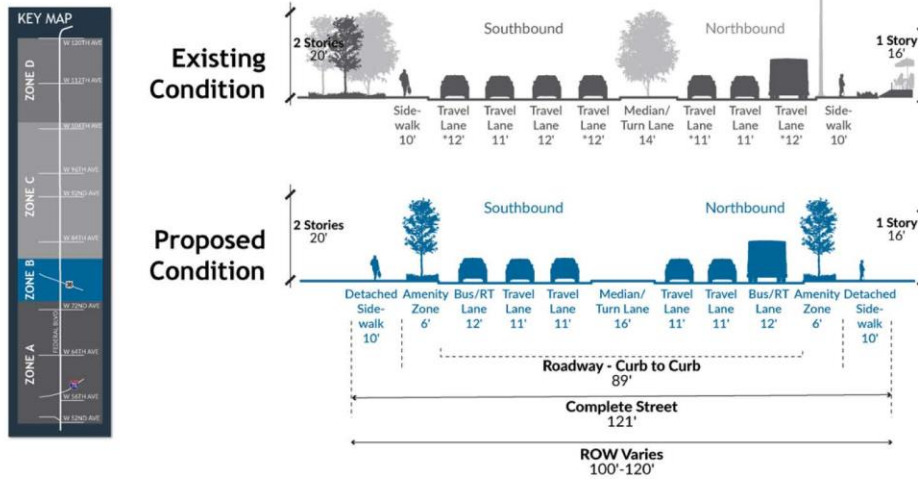
Zone A

Zone A Representative Location: Between 65th Avenue and Longfellow Place



Zone B

Zone B Representative Location: Between 73rd Avenue and 74th Avenue

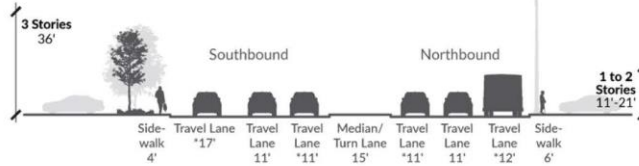


Zone C

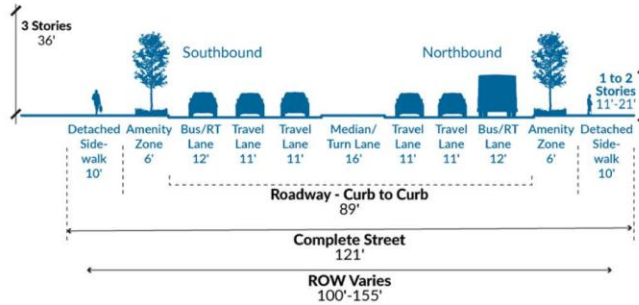
Zone C Representative Location: Between 91st Avenue and 91st Place



Existing Condition



Proposed Condition

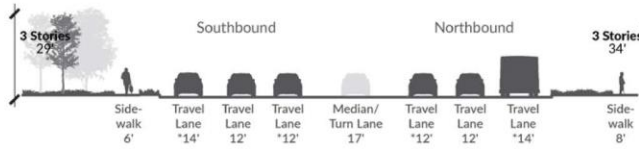


Zone D

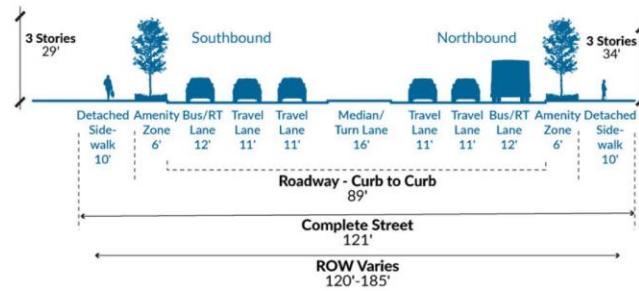
Zone D Representative Location: Between 107th Avenue and 108th Avenue



Existing Condition



Proposed Condition



Key Takeaways

Right-of-Way (ROW) and Other Impacts

- Most parcels are affected for typically minor ROW acquisitions
 - Varies along corridor
 - Most ROW needs impact parking to some extent
 - Few structures are impacted
- Modifications to interchanges and bridges will be needed

Stakeholder Feedback - Menti

Go to [menti.com](https://www.menti.com) and use the code: 30 69 96 5

Breakout Groups

Breakout Group Instructions

Instructions

- Discussion (25 min.)
 - React to Menti results
 - Discuss your thoughts on cross-section and right-of-way impacts
- Report back



Stakeholder Breakout Groups

We'd like your input

- Introductions
 1. Were the Menti results surprising to you? Why or why not?
- Review corridor impacts by zone
- Discussion
 1. Are the type and nature of impacts acceptable to achieve a *multimodal complete street* cross-section?
 2. Do you feel that the transit improvements shown in the cross section would help meet the transit goals for the corridor?
 3. What technical data do you need to support this cross section?
 4. Do you have any additional questions/comments?

Report Back

Next Steps

Upcoming

- Digital Survey
- Online public meeting



Thank You!